

METROPOLITAN ATLANTA RAPID TRANSIT AUTHORITY



CLAYTON COUNTY HIGH CAPACITY TRANSIT UPDATE

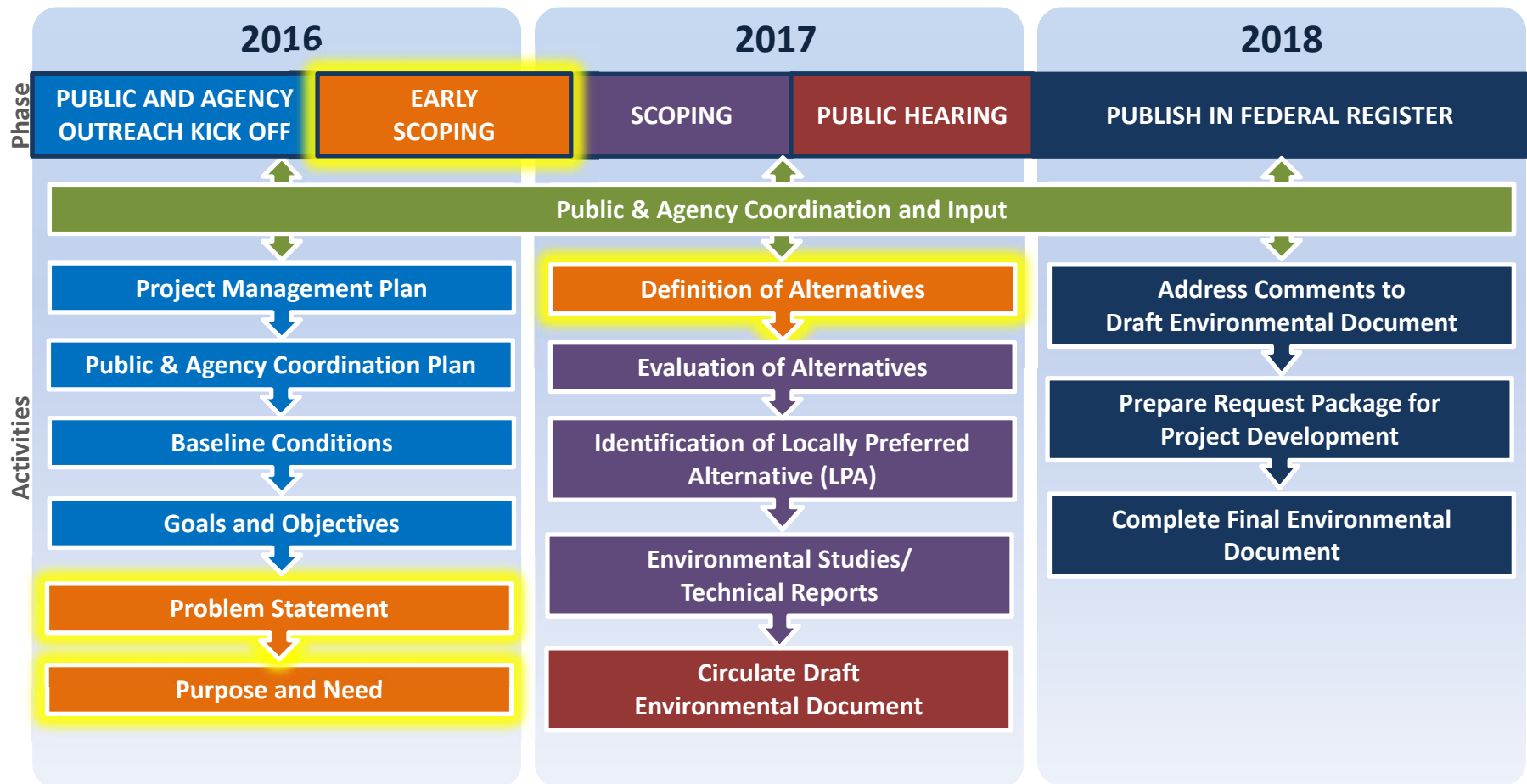
Technical Advisory Committee
MARTA Board Lounge
January 17, 2017

Agenda

- I. Welcome and Introductions
- II. Project Schedule
- III. Summary of Kick-Off Public and Agency Outreach
- IV. Purpose and Need/Goals and Objectives
- V. Potential Transit Corridors
- VI. Evaluation Process and Screening Criteria
- VII. Group Exercise
- VIII. Next Steps

PROJECT SCHEDULE

Project Schedule



Purpose of Early Scoping

- Optional step for community involvement during planning phase
- Encouraged by Federal Transit Administration (FTA) to streamline process
- Input and comments considered in the formal environmental review process
- Refines the project definition, identifies stakeholders, potential issues and concerns
- Helps narrow the range of transit alternatives in a large study area

SUMMARY OF KICK-OFF PUBLIC AND AGENCY OUTREACH

Kick Off Public and Agency Outreach Meetings

- Technical Advisory Committee (TAC)
 - October 25th - MARTA Bid Room
- Stakeholder Advisory Committee (SAC)
 - October 27th – Clayton County Board of Commissioners Office
- Public Meetings
 - November 10th – Riverdale Town Center
 - November 17th – Clayton State University



Key Themes from Public

- Schedule for completion of the study
- Expected date of implementation of high capacity transit service
- Frequency and on-time performance of existing transit service
- Routing, transit mode, and end of line of the high capacity transit service
- Public infrastructure improvements, opportunities for input, and safety
- Bus stop amenities (i.e. benches, shelters, trash receptacles)

PURPOSE AND NEED/ GOALS AND OBJECTIVES

Baseline Conditions Key Themes

- Continued population and employment growth
- Increased demand on transportation networks to access jobs
- Increased traffic congestion - Tara Blvd, SR85, SR54, SR138 & I-75
- Growing demand for transit for local and regional trips
- Transit supportive development is needed along major corridors
- Most development activity occurring in northern half of County

Transportation Problem Statements

- Mobility and connectivity in Clayton County will be limited by the following:
 - Population and employment growth that increases travel demand
 - Lack of transit service to potential transit oriented developments (TODs), redevelopment areas, and activity centers
 - Increased traffic congestion and delay
 - High number of traditional transit riders
 - High commute travel demand
 - Existing transit travel times are not competitive

Purpose and Need Statement

- The purpose of the Clayton High Capacity Transit Initiative Project is to:
 - Provide high capacity transit investments that:
 - Improve access to jobs
 - Enhance mobility
 - Accommodate increasing demand for transit
 - Support land use and economic development plans
 - Improve travel times and for commuters travelling to and from activity centers in the Atlanta Region and within Clayton County

Project Need

- Greater Access to Jobs and Education
- Additional Travel Options
- Regional Connections
- Growing Demand for Transit
- Support Land Use & Economic Development



Goals and Objectives

- Goal 1: Mobility and Access
 - Improve travel times and connectivity within Clayton County and to/from the Atlanta region
- Goal 2: Support Land Use Plans
 - Support transit supportive land uses identified in local land use plans
 - Support TOD initiatives
- Goal 3: Economic Development
 - Support economic development and redevelopment areas



Goals and Objectives


- Goal 4: Cost Effectiveness
 - Provide high capacity transit service that can be implemented with existing funding sources
 - Increase benefits to transportation system users
- Goal 5: Sustainability
 - Provide high capacity transit service that can be operated and maintained with existing and future resources
 - Minimize impacts to the natural and built environments as well as social and cultural resources

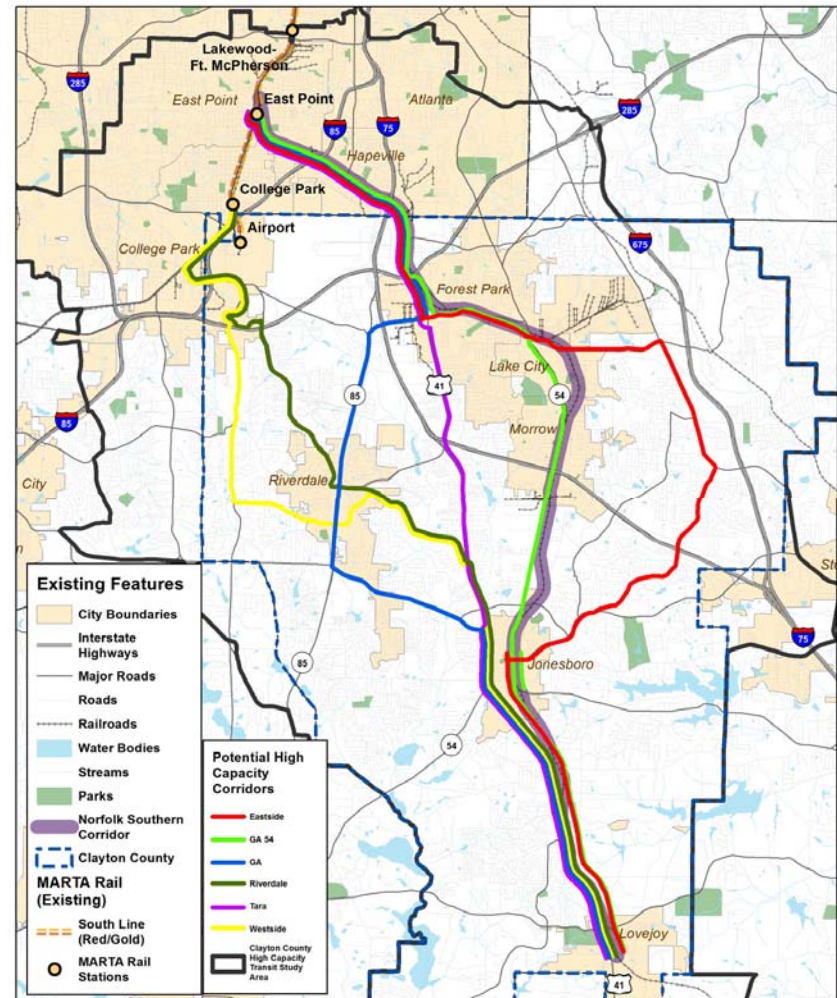
POTENTIAL TRANSIT CORRIDORS

Previous Studies/Analysis

- *Clayton Extension Report* (June 2014)
- *Clayton County High Capacity Transit – Conceptual Planning Technical Memorandum* (September 2014)
- Connect existing MARTA rail system to Lovejoy in Clayton County
- Six potential corridors evaluated for high capacity transit
- Norfolk Southern rail corridor involves continued coordination between MARTA and railroad company

Potential Transit Corridors

-  Westside Corridor
-  Riverdale Corridor
-  SR 85 Corridor
-  Tara Blvd Corridor
-  SR 54 Corridor
-  Norfolk Southern Corridor
-  Eastside Corridor



Potential Transit Technologies

Commuter Rail Transit (CRT)

Typical Characteristics

Seated capacity (per train):	250 - 1,000 passengers
Service range:	Up to 50 miles
Average speed:	35 - 45 mph
Station spacing:	2 - 4 miles apart
Alignment:	At-grade, exclusive guideway/freight corridor
Example Systems:	Austin, TX; Chicago, IL; Seattle, WA

Heavy Rail Transit (HRT)

Typical Characteristics

Seated capacity (per train):	500 passengers
Service range:	Up to 30 miles
Average speed:	30 - 35 mph
Station spacing:	Greater than 1 mile apart
Alignment:	Fully grade-separated fixed guideway
Example Systems:	Atlanta, GA; San Francisco, CA; Washington, DC



Potential Transit Technologies

Light Rail Transit (LRT)

Typical Characteristics

Seated capacity (per train):	225 passengers
Service range:	Up to 15 miles
Average speed:	25 - 35 mph
Station spacing:	0.5 - 1 mile apart
Alignment:	Exclusive/ semi-exclusive guideway
Example Systems:	Charlotte, NC; Dallas, TX; Minneapolis, MN



Bus Rapid Transit (BRT)

Typical Characteristics

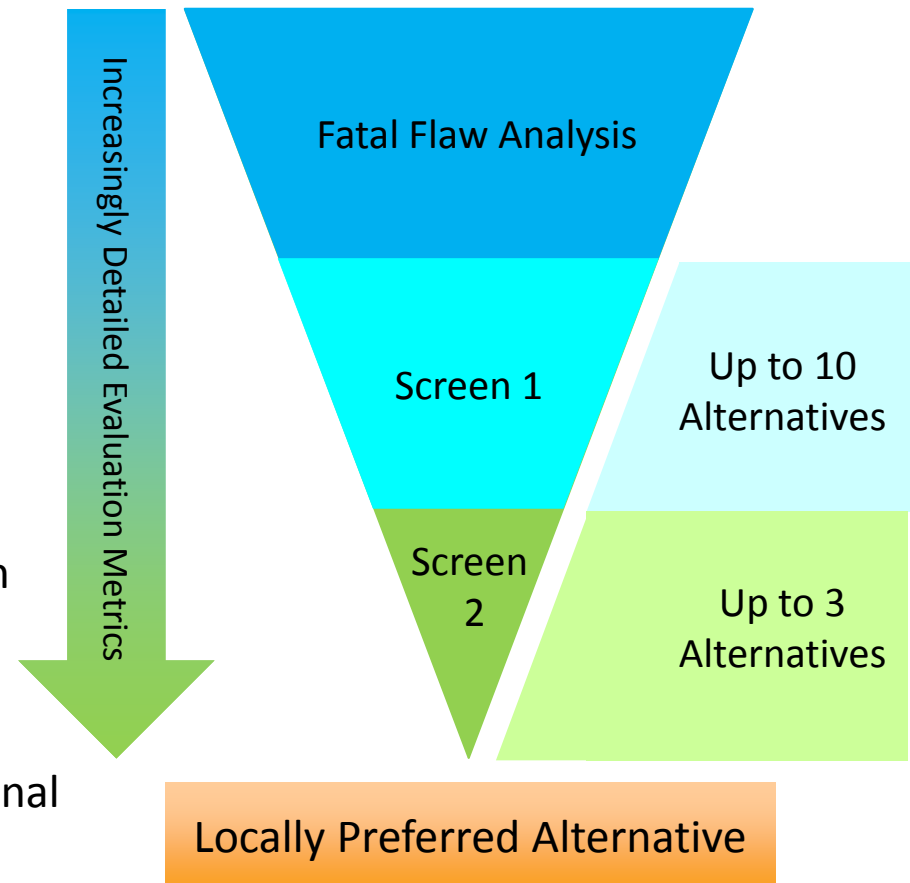
Seated capacity (per bus):	40 - 60 passengers
Service range:	Up to 15 miles
Average speed:	20 - 30 mph
Station spacing:	0.25 - 0.5 mile apart
Alignment:	Semi-exclusive guideway
Example Systems:	Cleveland, OH; Eugene, OR; Los Angeles, CA



EVALUATION PROCESS AND SCREENING CRITERIA

Evaluation Process

- Fatal Flaw Analysis
 - Community input
 - Purpose & need
 - Constructability/connectivity
 - Right-of-way impacts
 - Technology assessment
- Screen 1
 - Quantitative & qualitative evaluation criteria
- Screen 2
 - More in-depth analysis using additional performance measures



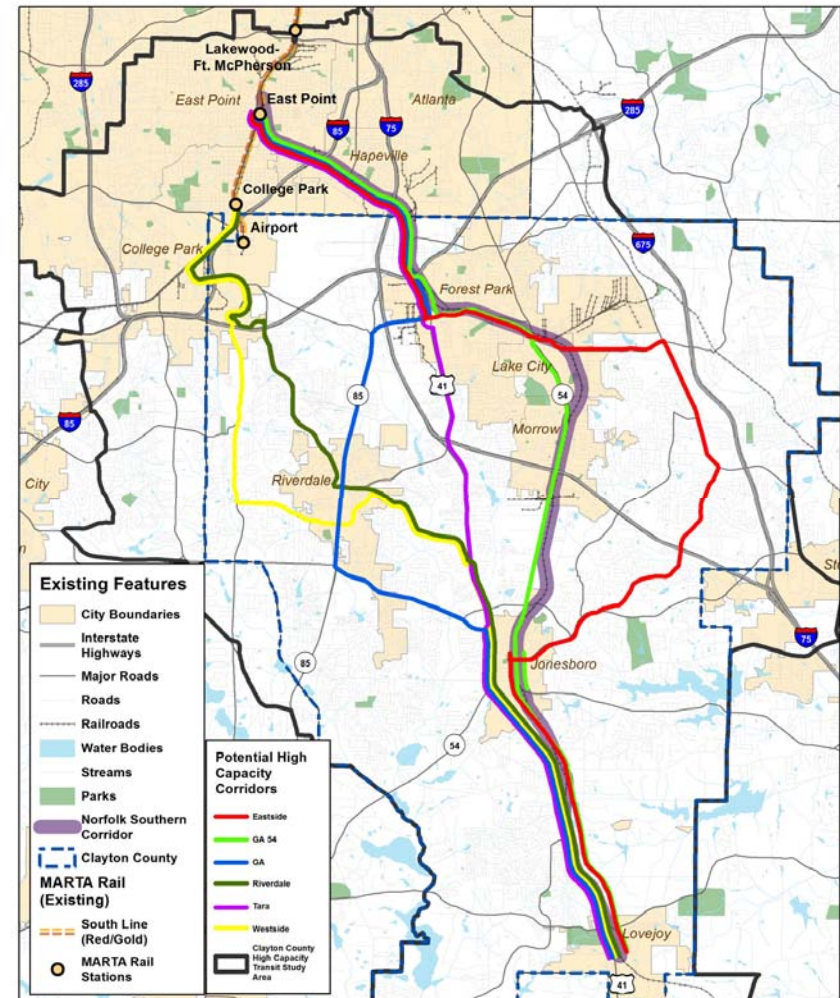
Screening Criteria

- Goal 1: Mobility and Access
 - Trip Time Reduction
 - Congestion Relief
 - Serve Strongest Travel Patterns
 - Increase Connectivity
 - Total Daily Boardings
 - New System Transit Trips
- Goal 2: Support Land Use Plans
 - Transit-Supportive Land Use
 - Transit-Oriented Development Potential
- Goal 3: Economic Development
 - Development Potential
- Goal 4: Cost Effectiveness
 - Operating and Maintenance Costs
 - Capital Costs
 - Transportation System User Benefits
- Goal 5: Sustainability
 - Displacements
 - Noise
 - Cultural or Natural Resources
 - Water Resources

GROUP EXERCISE

Rate the Potential Transit Corridors

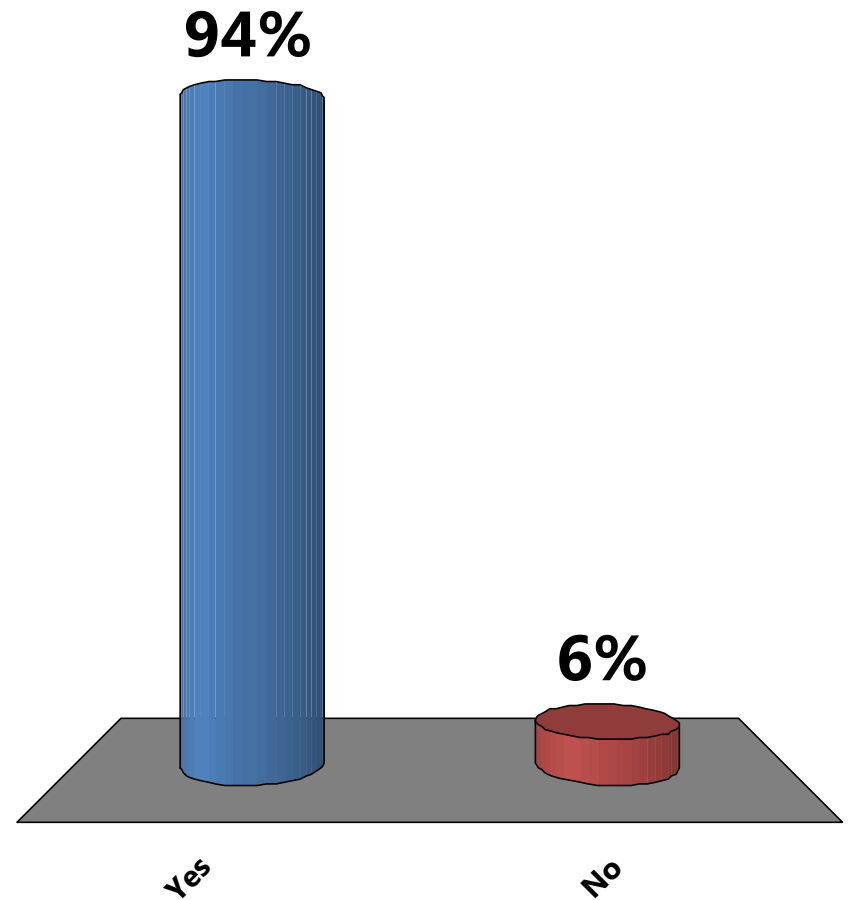
- Provide your input to determine how each corridor addresses project needs:
 - Connectivity to major destinations
 - Serves traditional transit markets
 - Serves commuter transit markets
 - Provides access for pedestrians, bicyclists and/or motorists



Group Exercise

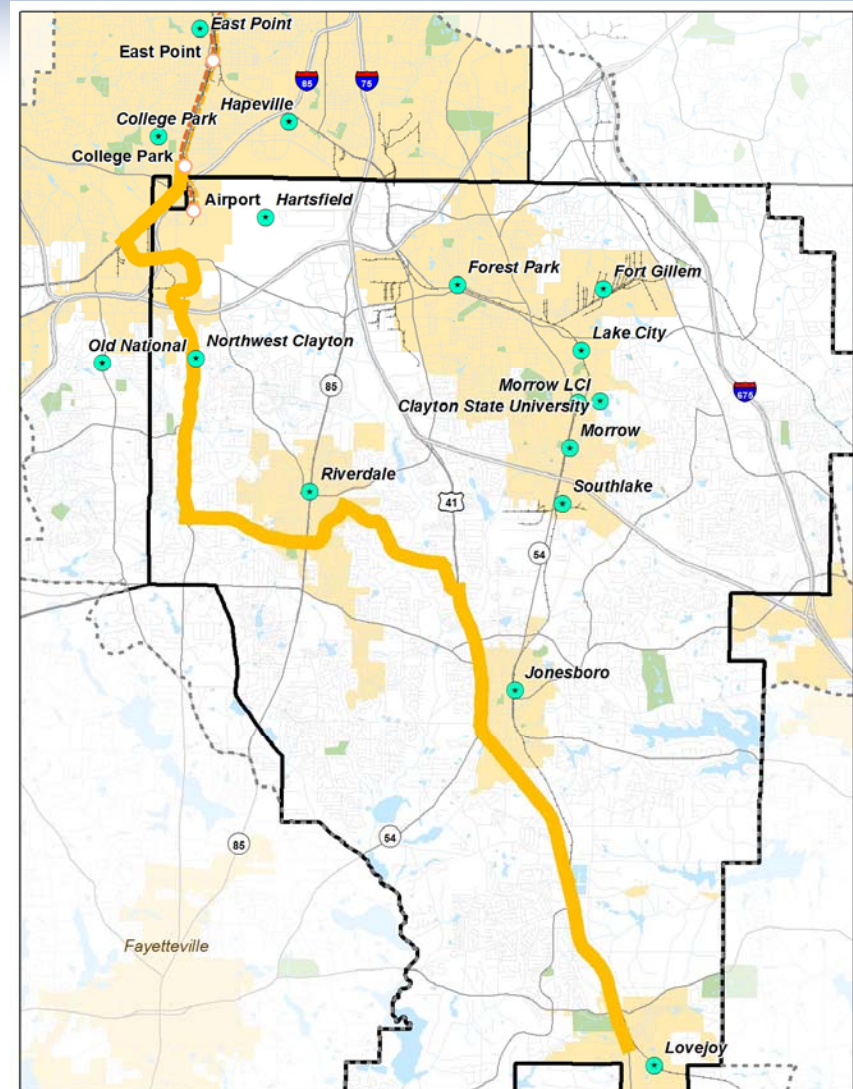
- Do you like ice cream?

1. Yes
2. No



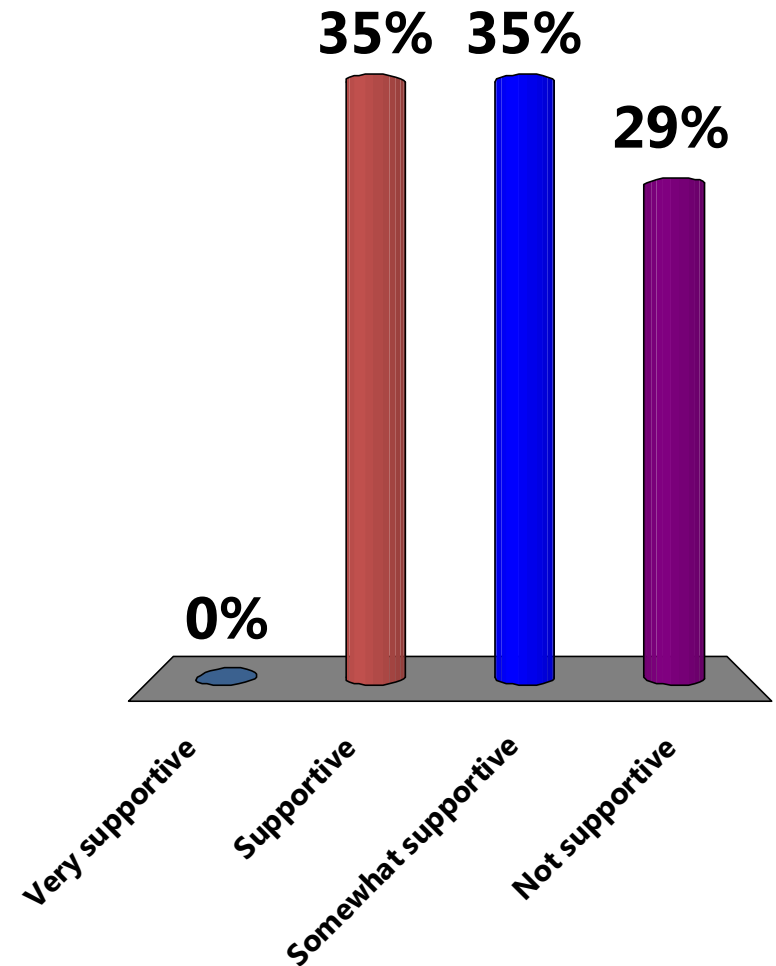
WESTSIDE CORRIDOR

- MARTA College Park Station
 - Roosevelt Hwy
 - Riverdale Rd
 - Hutcheson Pkwy
 - Fayetteville Rd
 - Bethsaida Rd
 - Valley Hill Rd
 - Tara Blvd
- Lovejoy



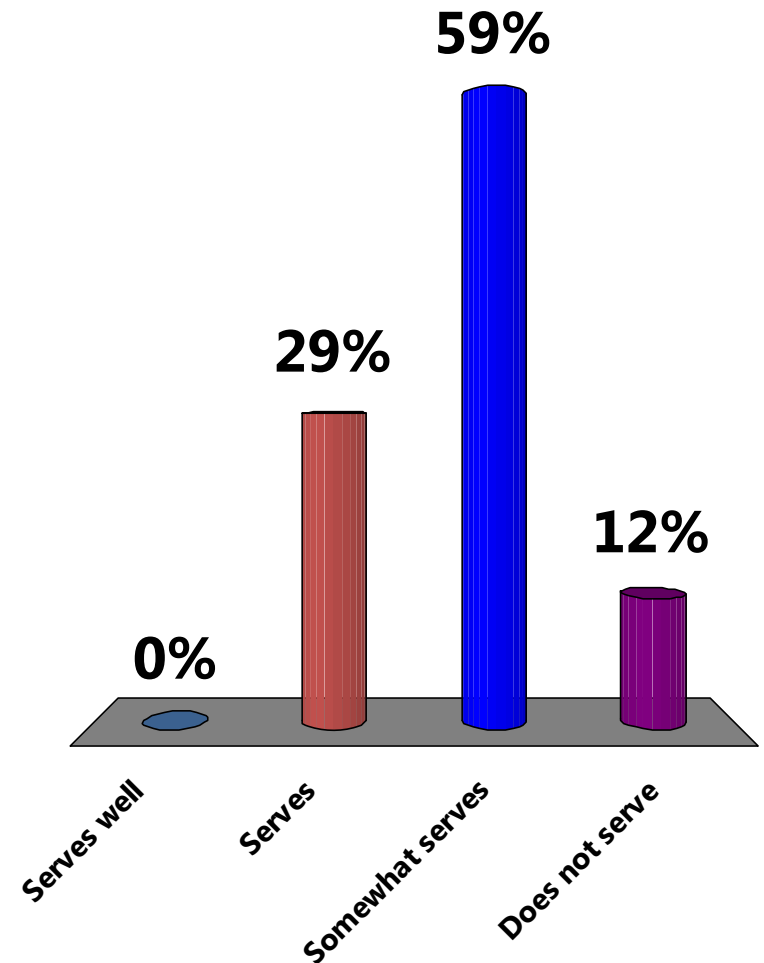
Group Exercise - Westside Corridor

- Is the Westside Corridor supportive of connections to key activity centers, growth areas and/or economic development initiatives?
 1. Very supportive
 2. Supportive
 3. Somewhat supportive
 4. Not supportive



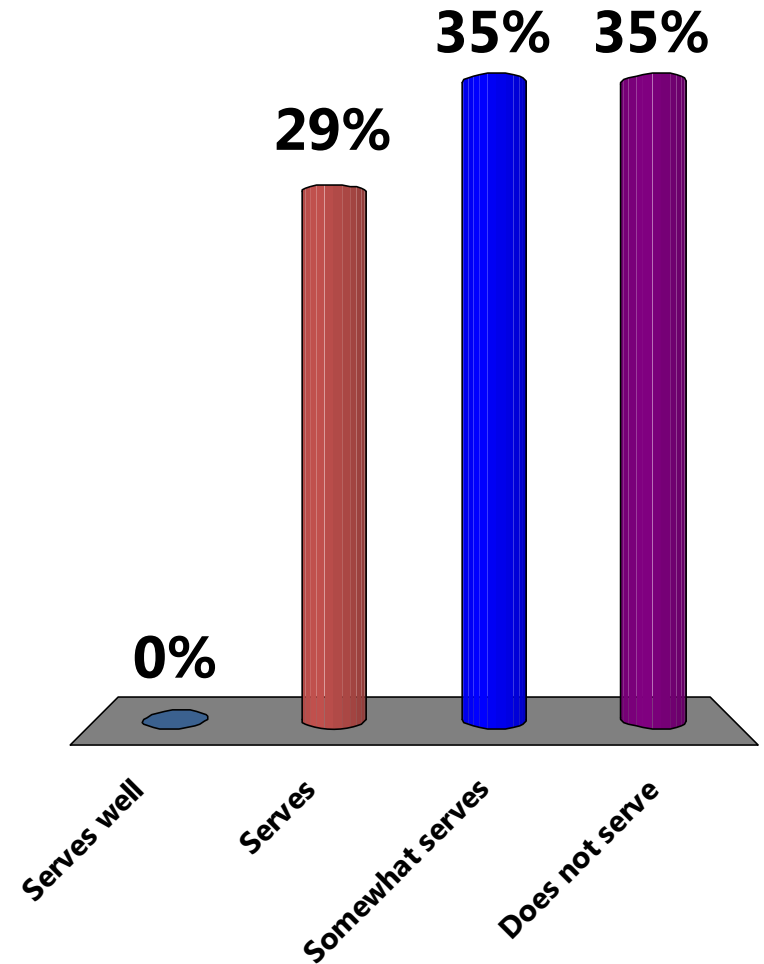
Group Exercise - Westside Corridor

- Does the Westside Corridor serve traditional transit markets (e.g. households without vehicles, non-drivers)
 1. Serves well
 2. Serves
 3. Somewhat serves
 4. Does not serve



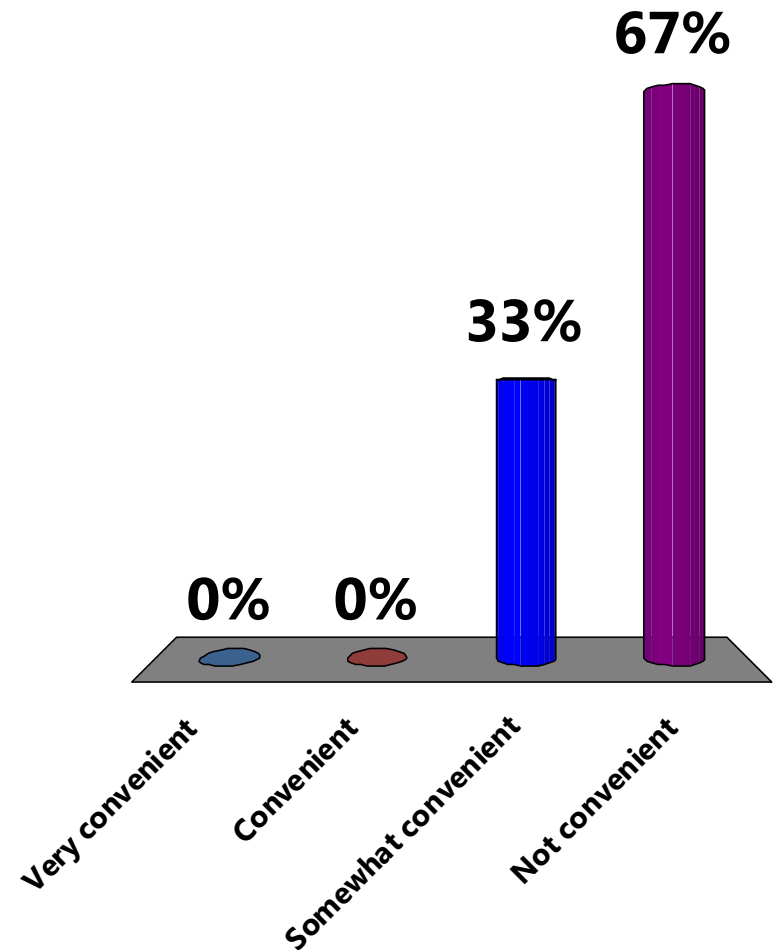
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- Does the Westside Corridor serve commuters?
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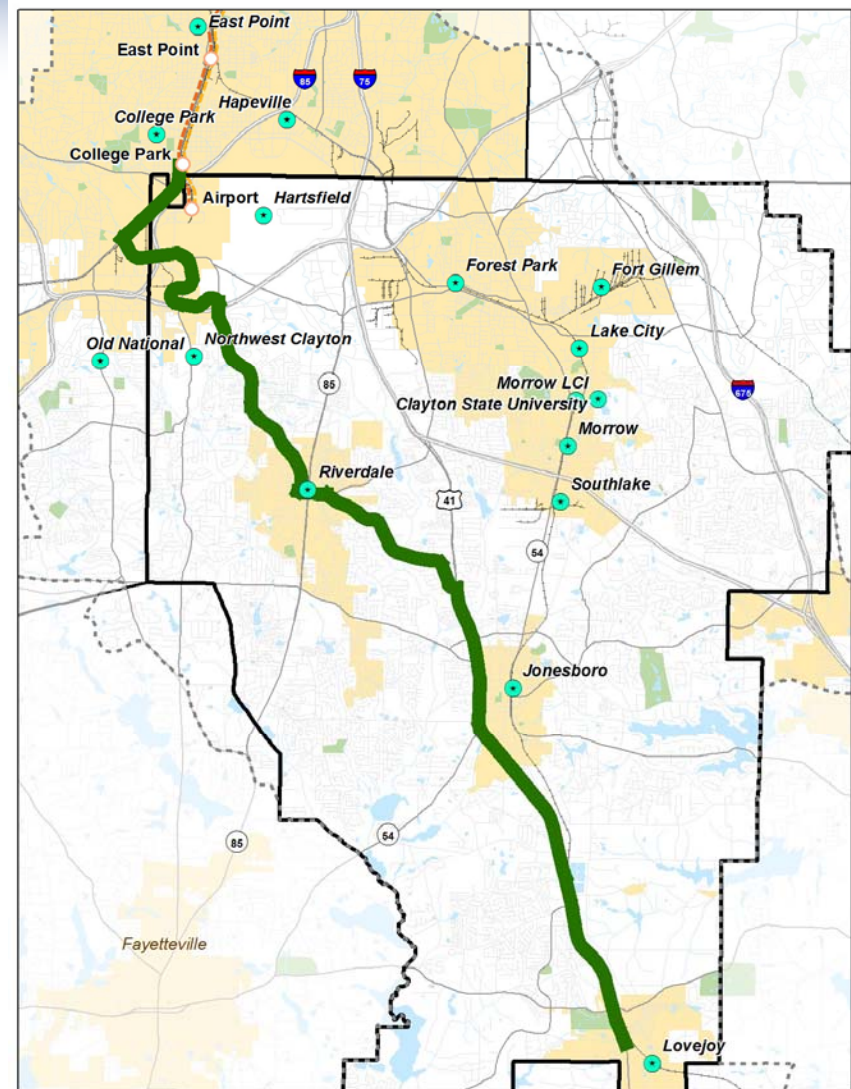
Group Exercise - Westside Corridor

- Does the Westside Corridor provide convenient access for pedestrians, bicyclists and/or motorists?
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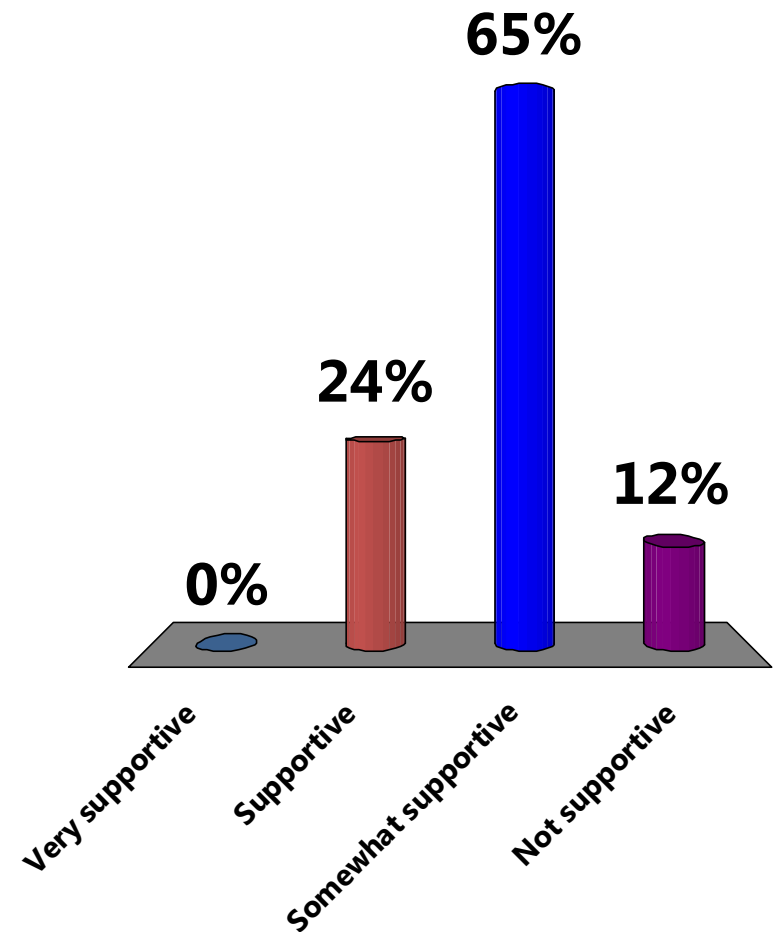
RIVERDALE CORRIDOR

- MARTA College Park Station
 - Roosevelt Hwy
 - Riverdale Rd
 - Valley Hill Rd
 - Tara Blvd
- Lovejoy



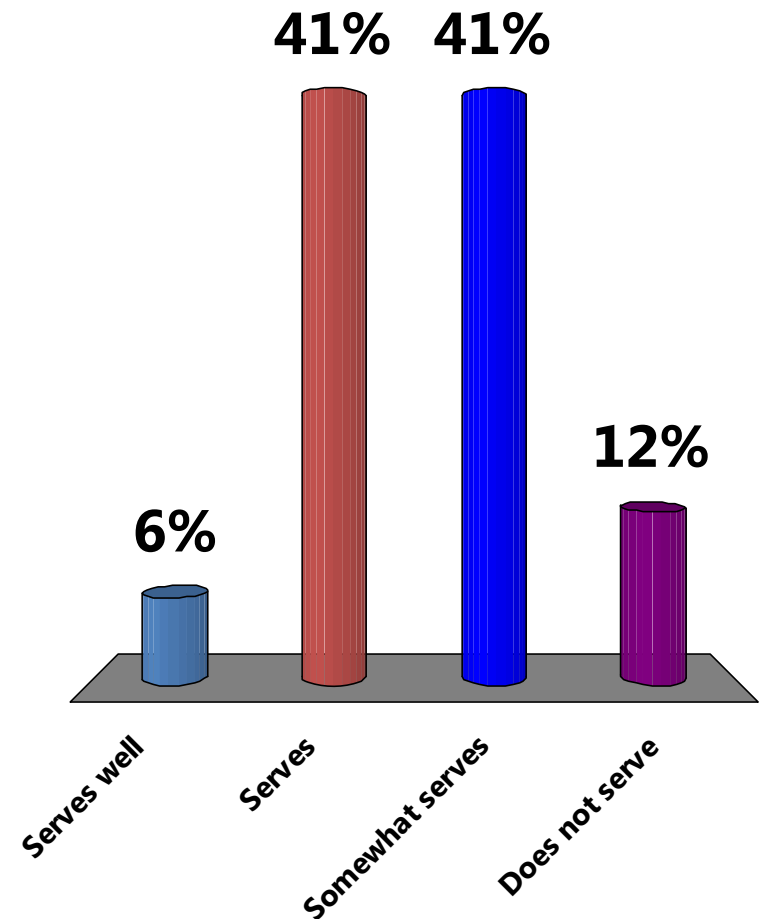
Group Exercise - Riverdale Corridor

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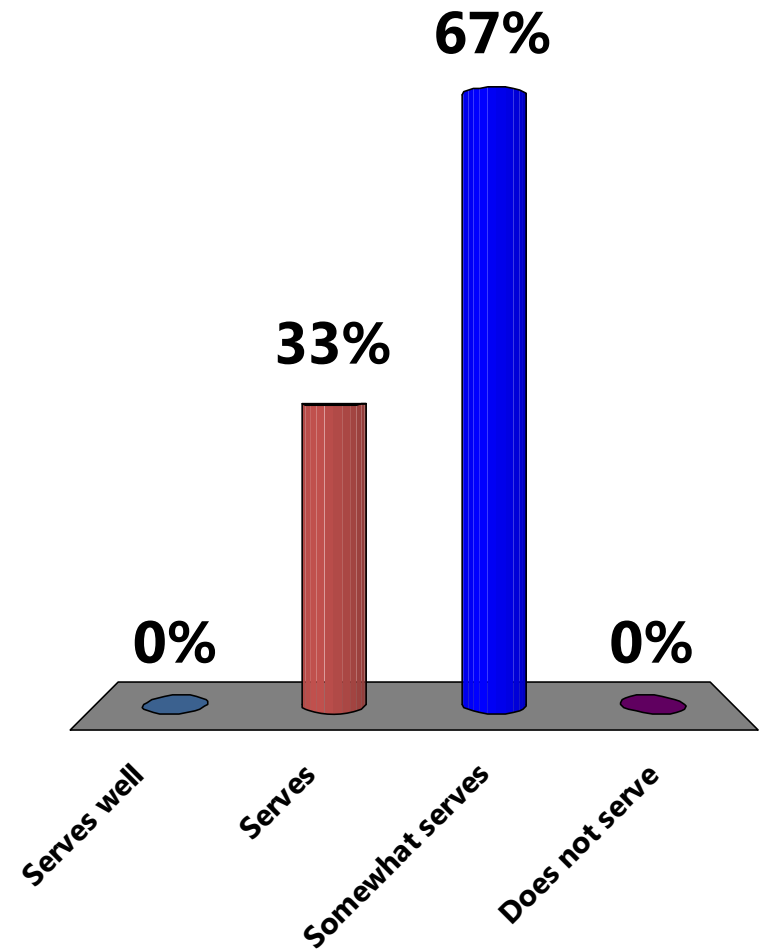
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 - Does not serve



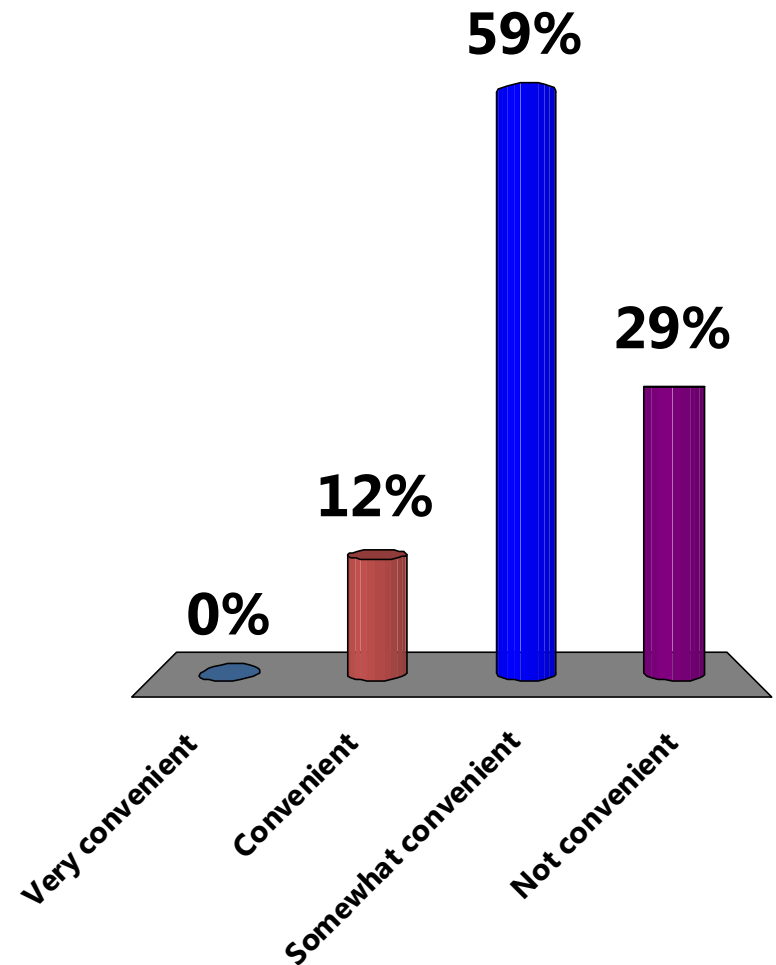
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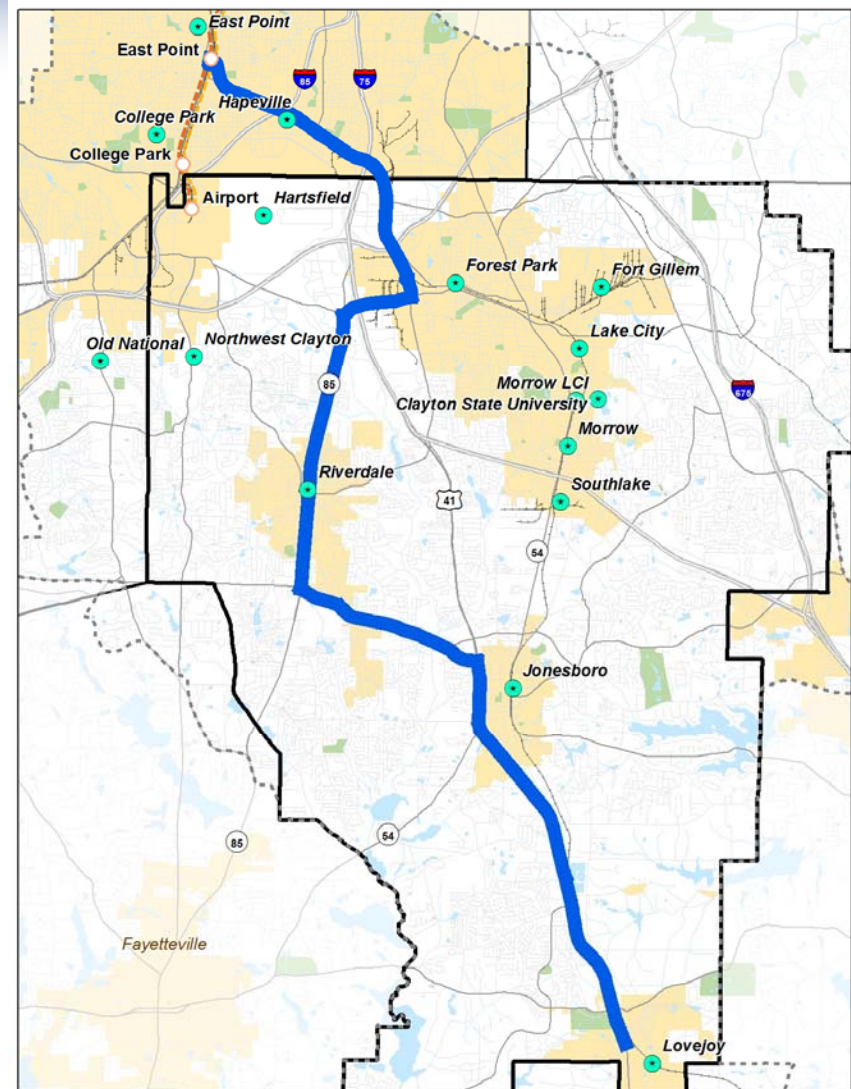
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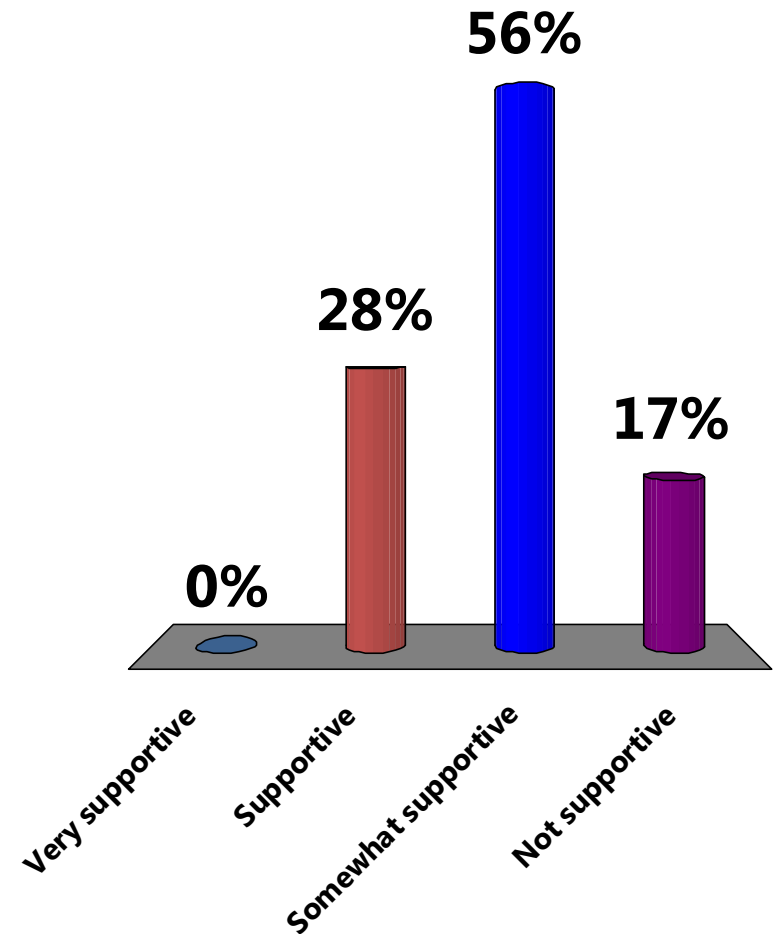
SR 85 CORRIDOR

- MARTA East Point Station
 - Central Ave
 - Old Dixie Rd
 - Forest Pkwy
 - GA 85
 - GA 138
 - Tara Blvd
- Lovejoy



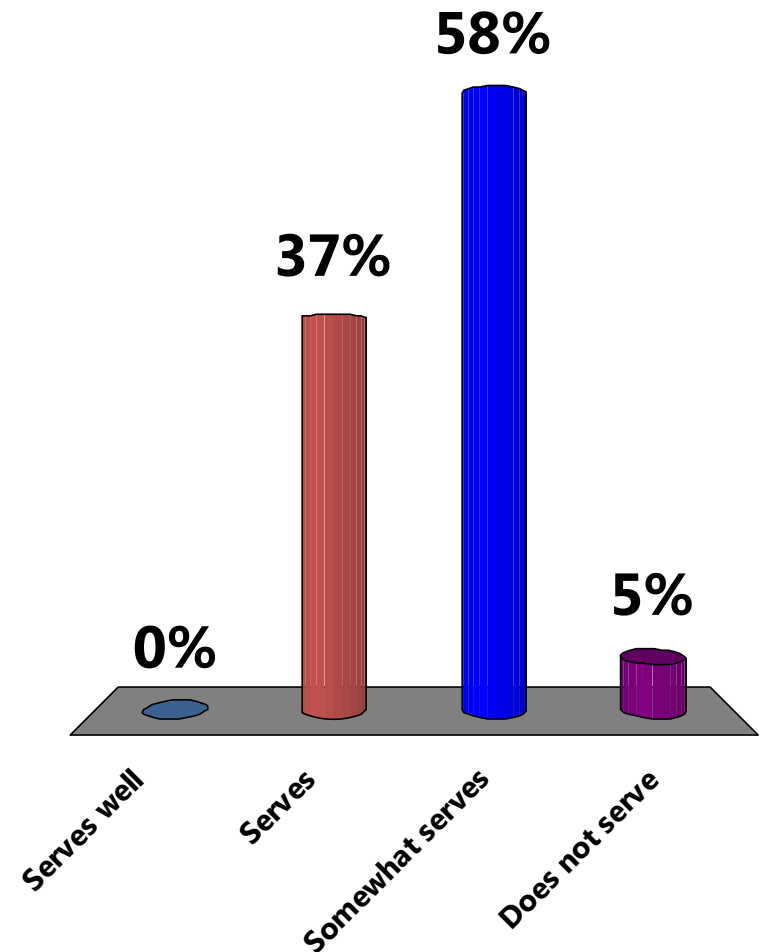
Group Exercise – SR 85 Corridor

- Is the SR 85 Corridor supportive of connections to key activity centers, growth areas and/or economic development initiatives?
 1. Very supportive
 2. Supportive
 3. Somewhat supportive
 4. Not supportive



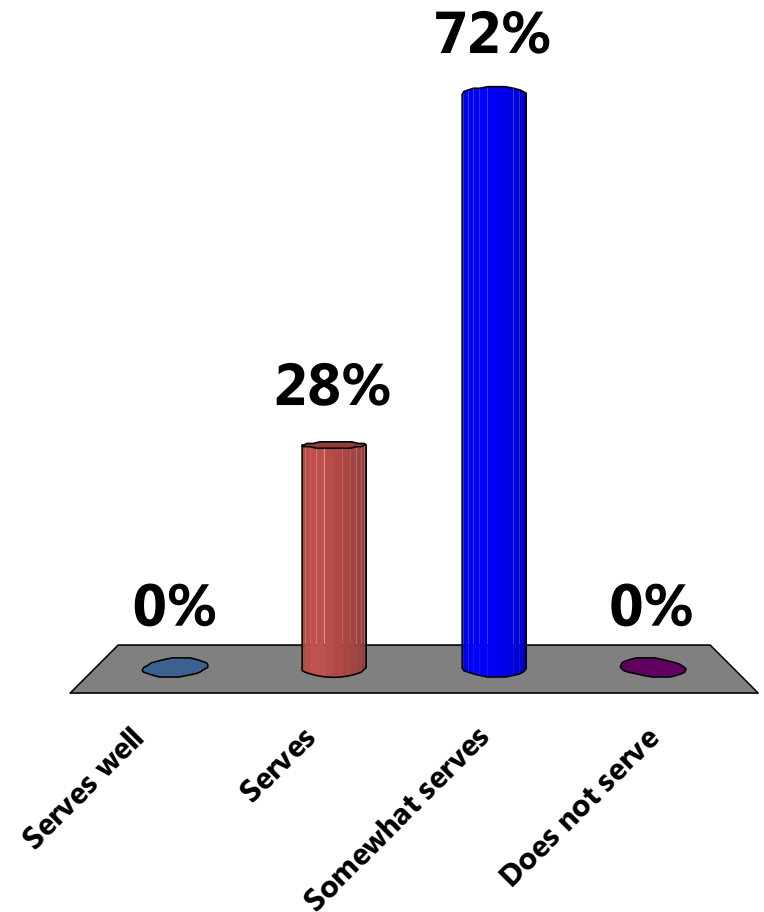
Group Exercise - SR 85 Corridor

- Does the SR 85 Corridor serve traditional transit markets (e.g. households without vehicles, non-drivers)
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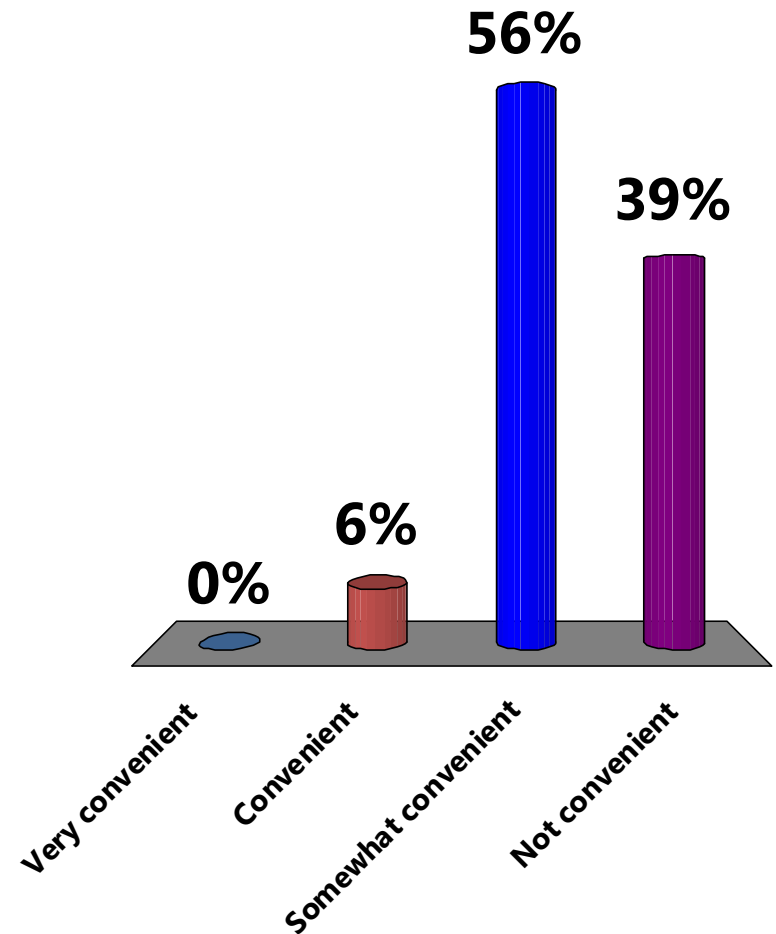
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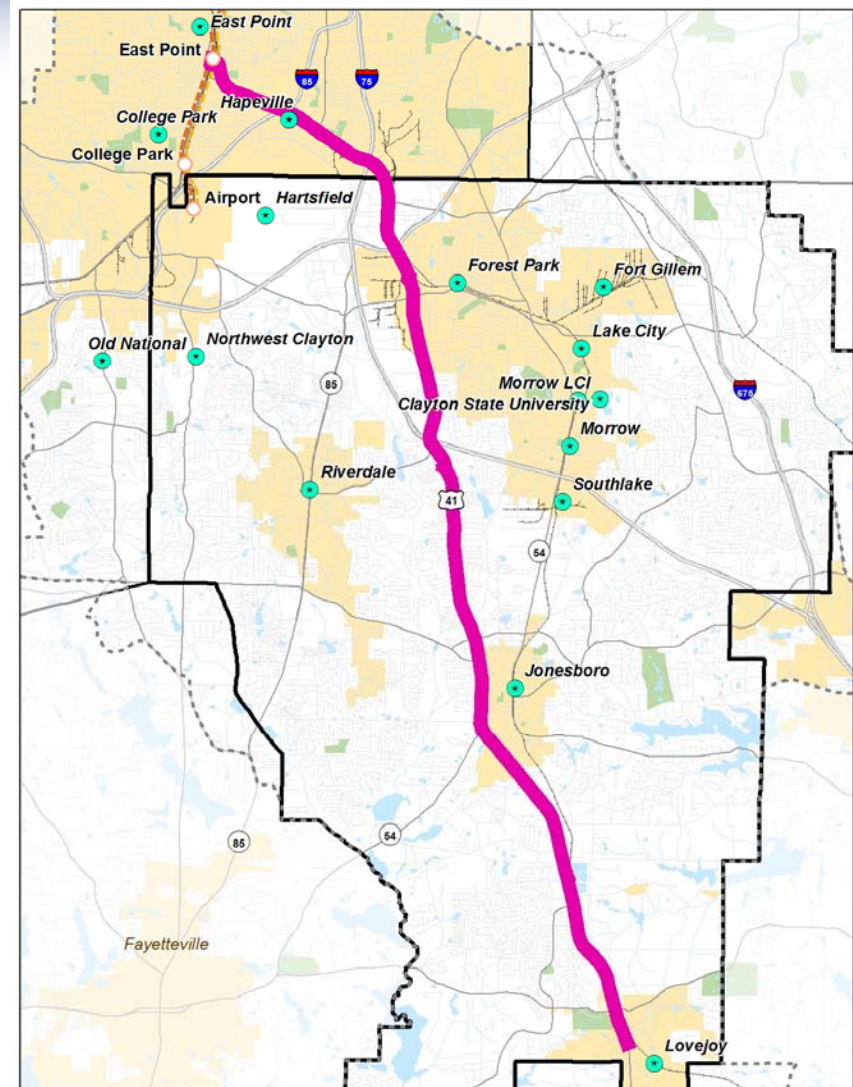
Group Exercise - SR 85 Corridor

- Does the SR 85 Corridor have provide convenient access for pedestrians, bicyclists and/or motorists?
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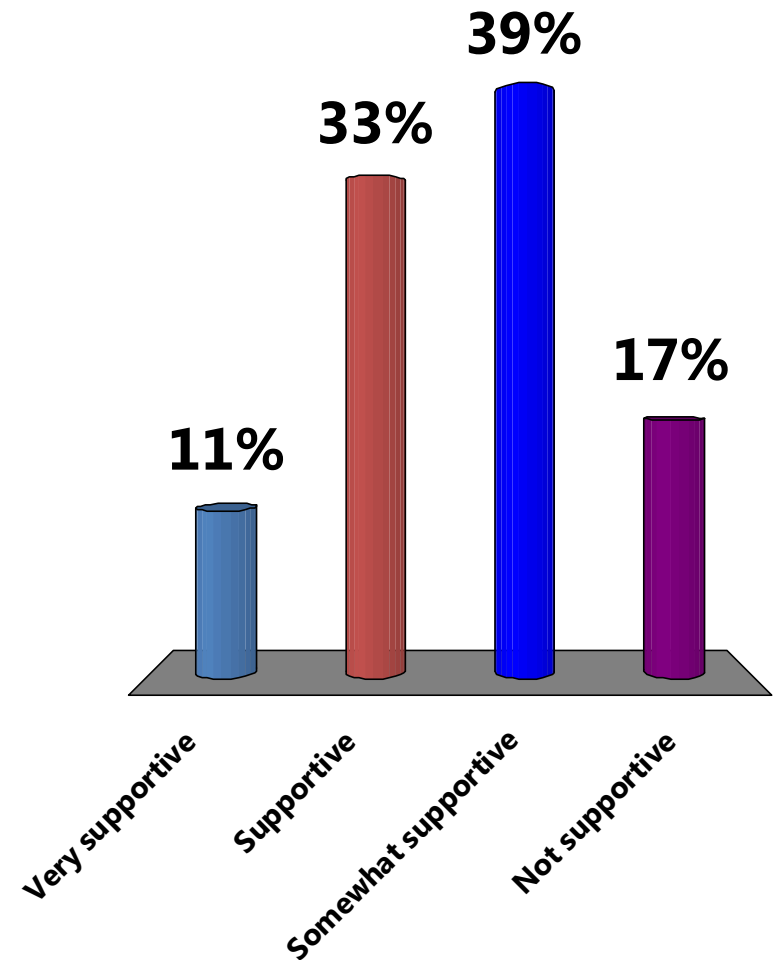
TARA BLVD CORRIDOR

- MARTA East Point Station
 - Central Ave
 - Old Dixie Rd
 - Tara Blvd
- Lovejoy



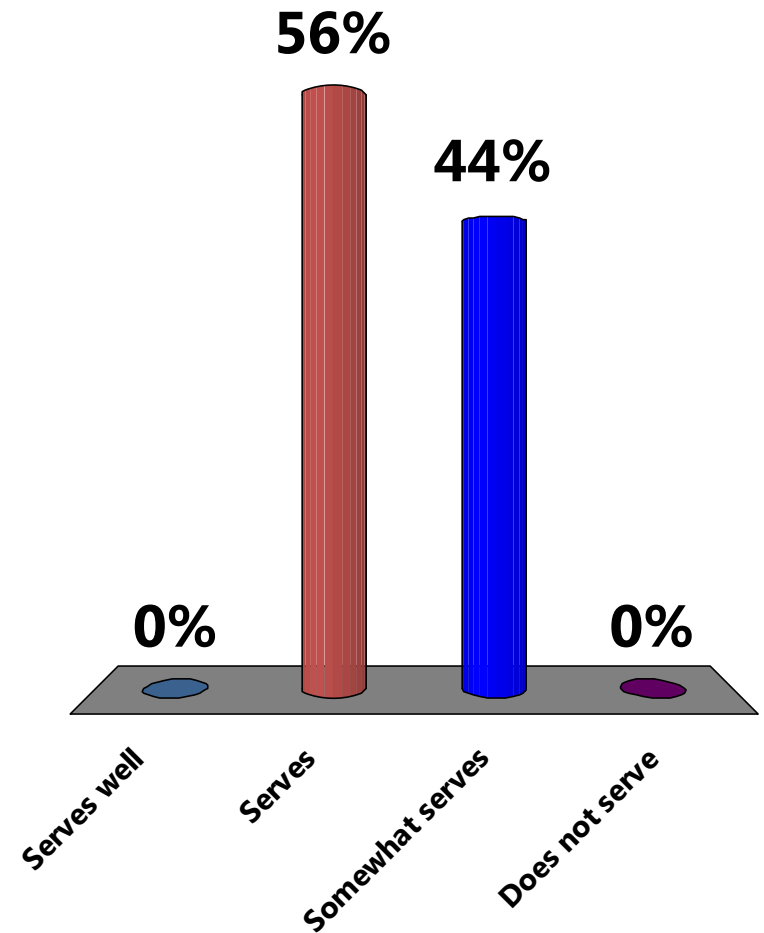
Group Exercise – Tara Blvd. Corridor

- Is the Tara Blvd. Corridor supportive of connections to key activity centers, growth areas and/or economic development initiatives?
 1. Very supportive
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 4. Not supportive



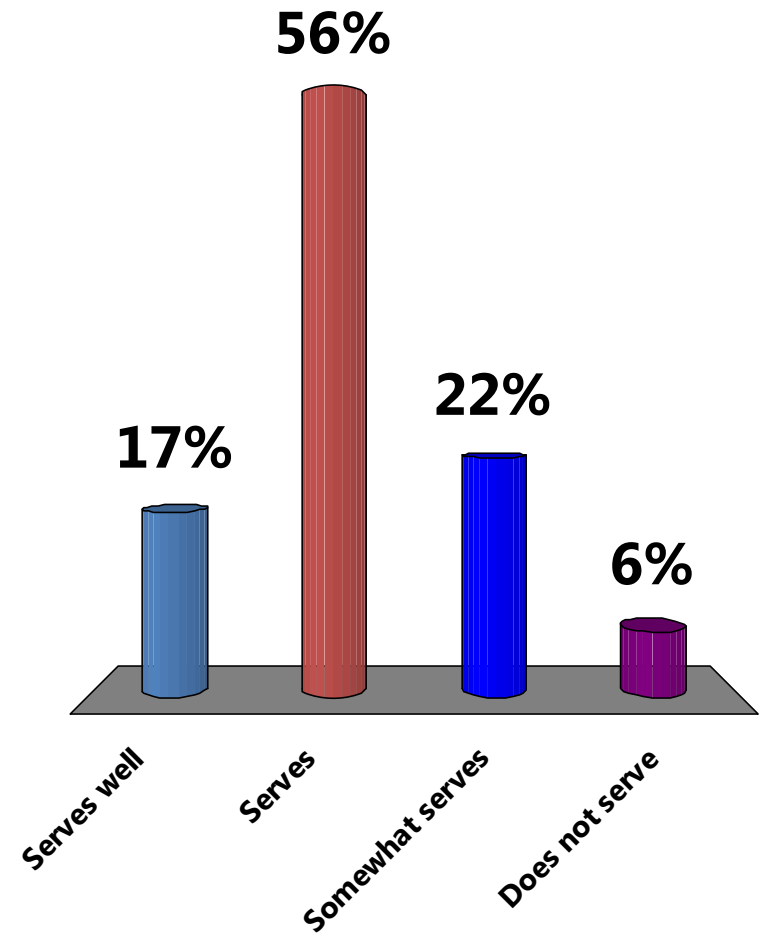
Group Exercise - Tara Blvd. Corridor

- Does the Tara Blvd. Corridor serve traditional transit markets (e.g. households without vehicles, non-drivers)
 1. Serves well
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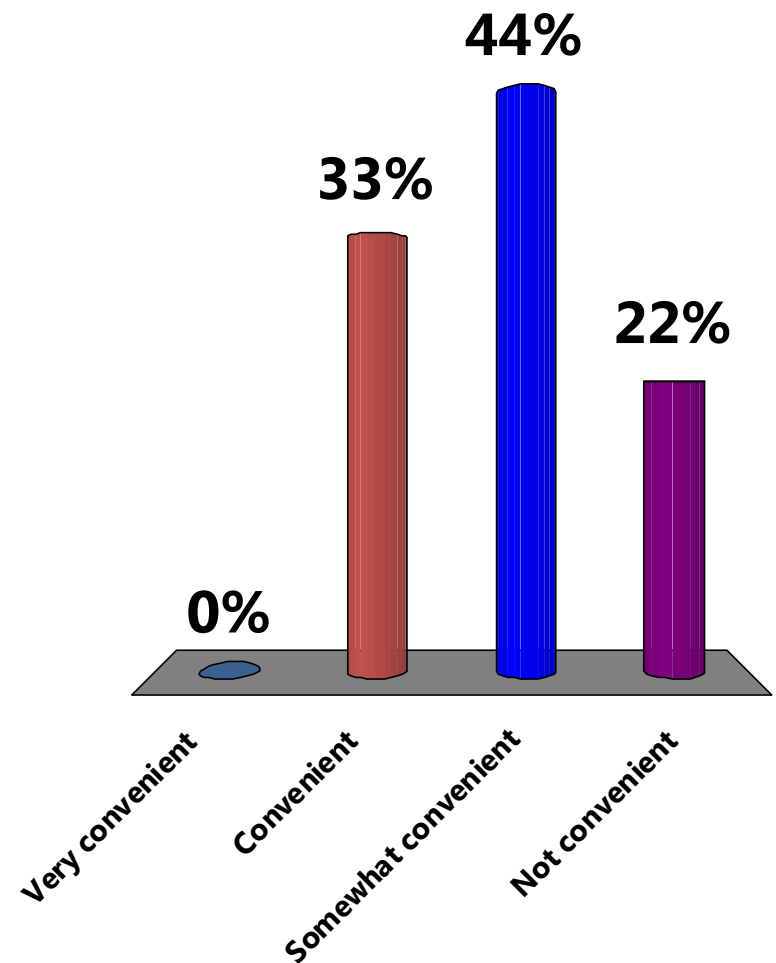
Group Exercise - Tara Blvd. Corridor

- Does the Tara Blvd. Corridor serve commuters?
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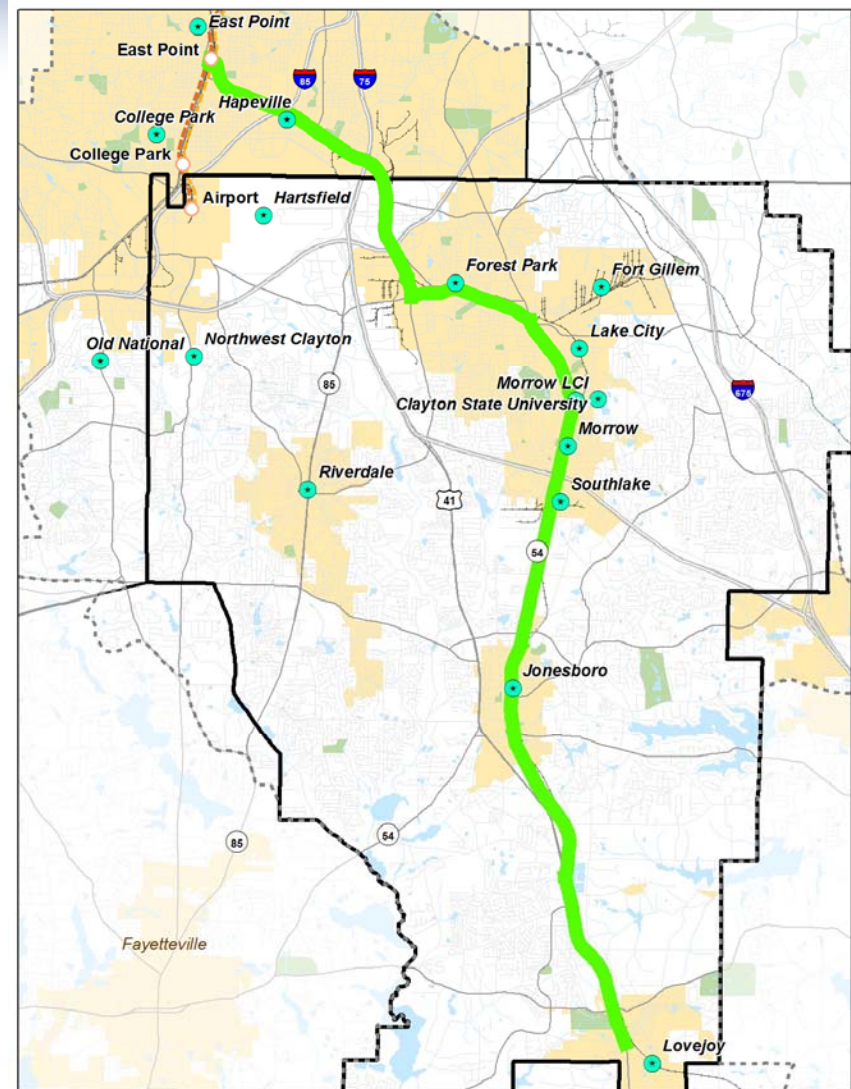
Group Exercise - Tara Blvd. Corridor

- Does the Tara Blvd. Corridor provide convenient access for pedestrians, bicyclists and/or motorists?
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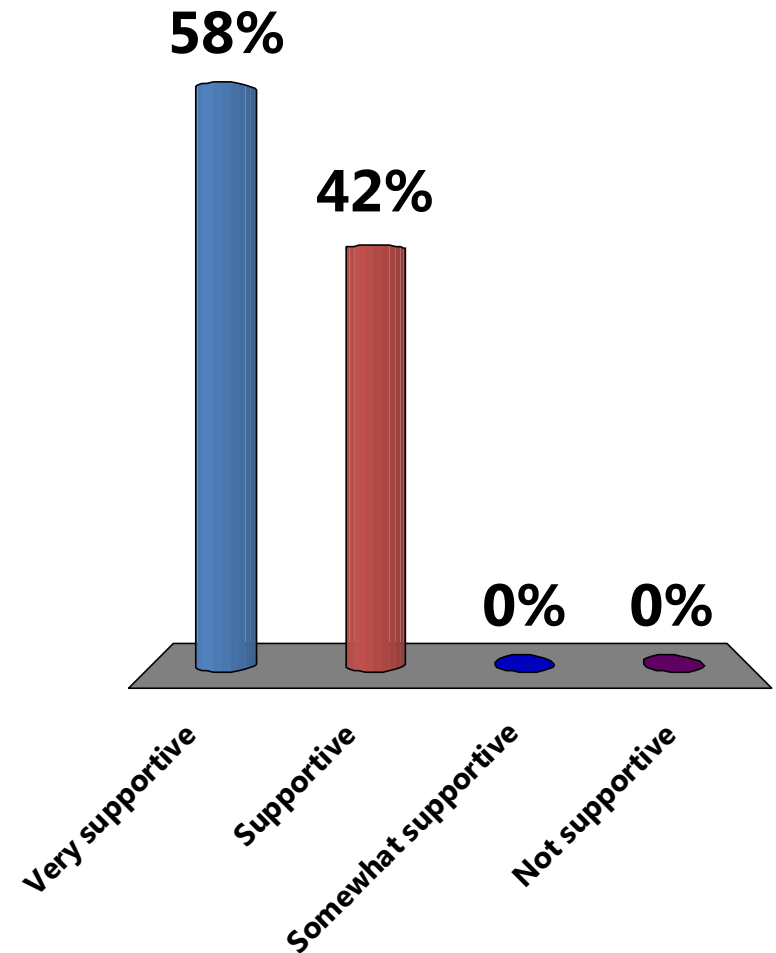
SR 54 CORRIDOR

- MARTA East Point Station
 - Central Ave
 - Old Dixie Rd
 - Forest Pkwy
 - Jonesboro Rd
 - Main St
 - Tara Blvd
- Lovejoy



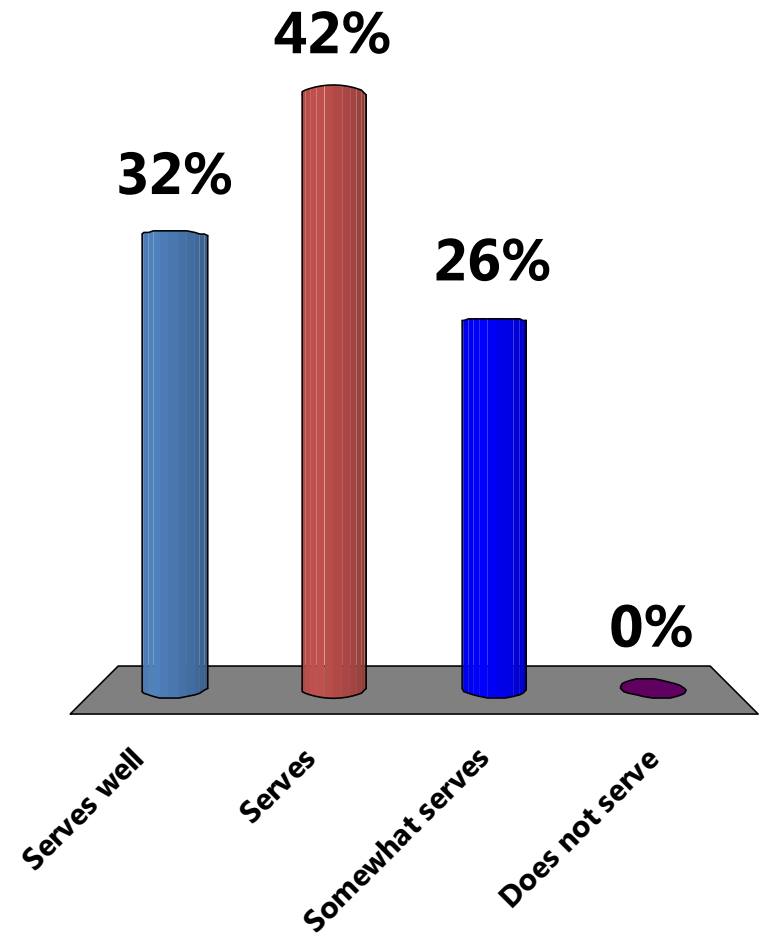
Group Exercise – SR 54 Corridor

- Is the SR 54 Corridor supportive of connections to key activity centers, growth areas and/or economic development initiatives?
 1. Very supportive
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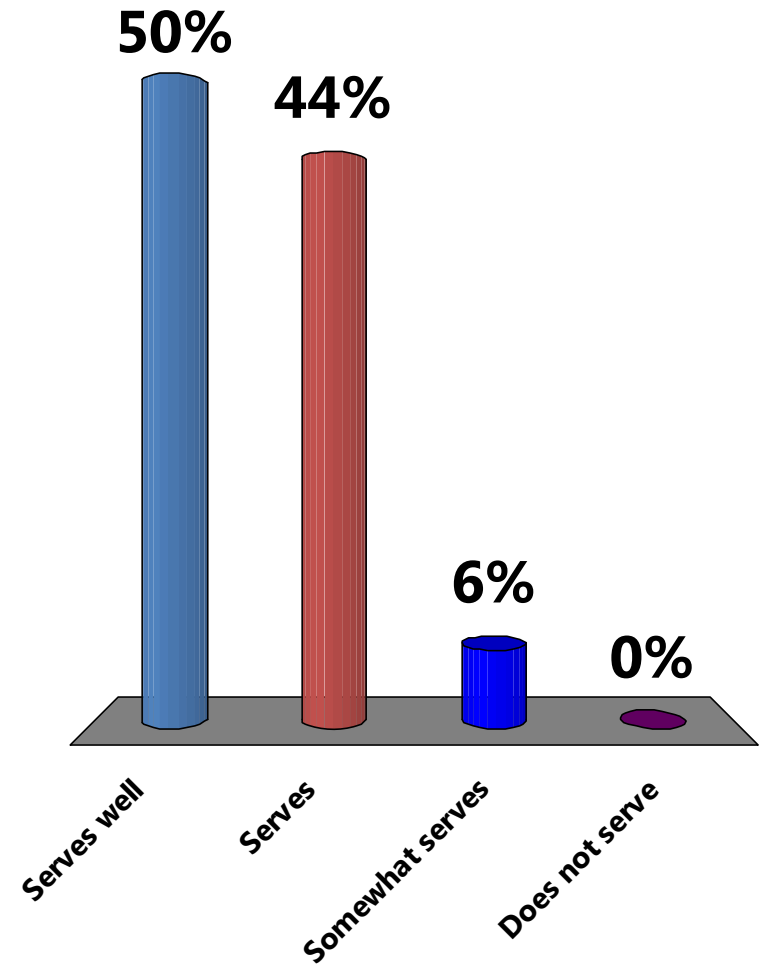
Group Exercise - SR 54 Corridor

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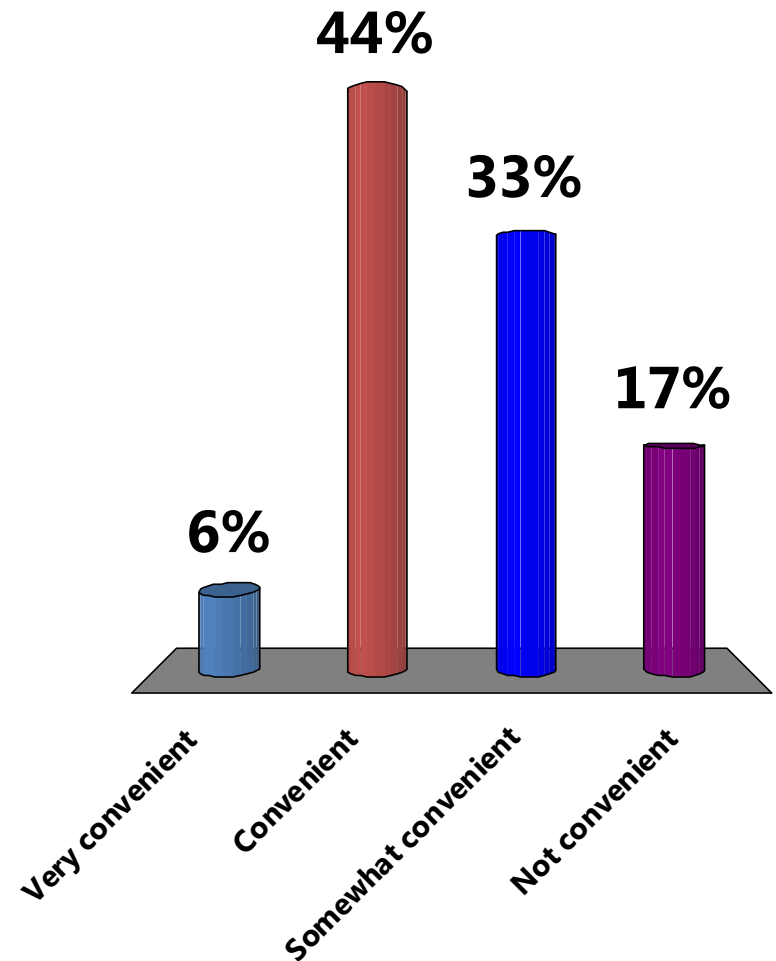
Group Exercise - SR 54 Corridor

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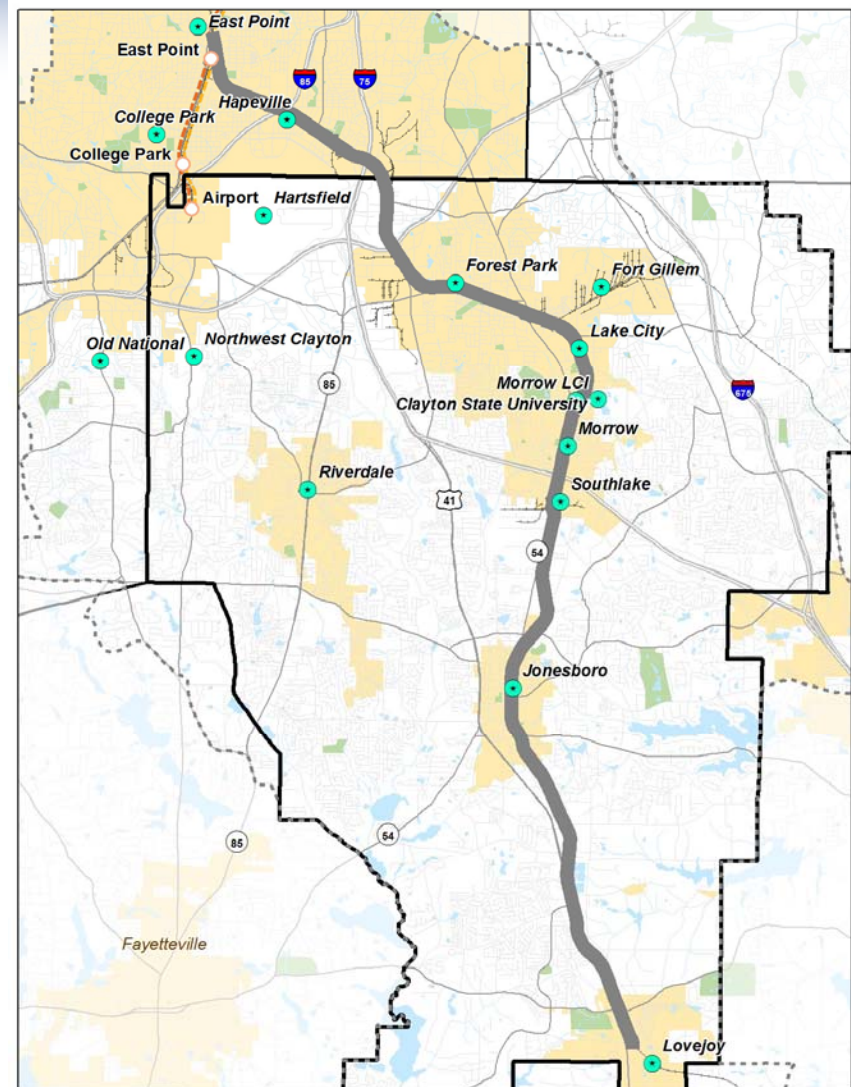
Group Exercise - SR 54 Corridor

- Does the SR 54 Corridor have provide convenient access for pedestrians, bicyclists and/or motorists?
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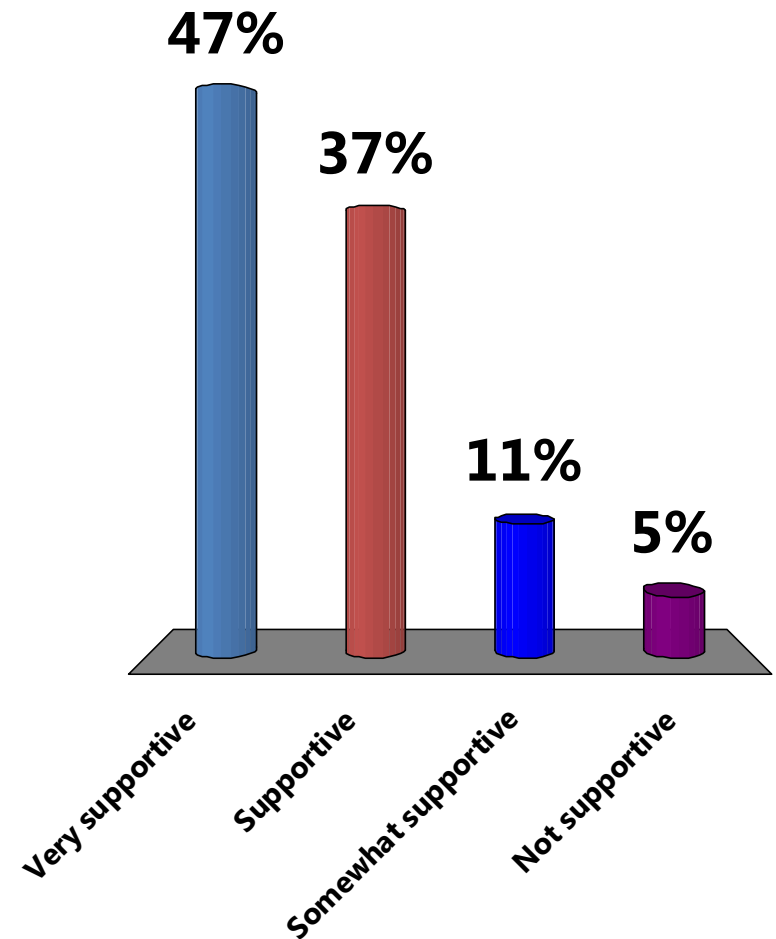
NORFOLK SOUTHERN CORRIDOR

- MARTA East Point Station
 - Norfolk Southern Corridor Right of Way
- Lovejoy



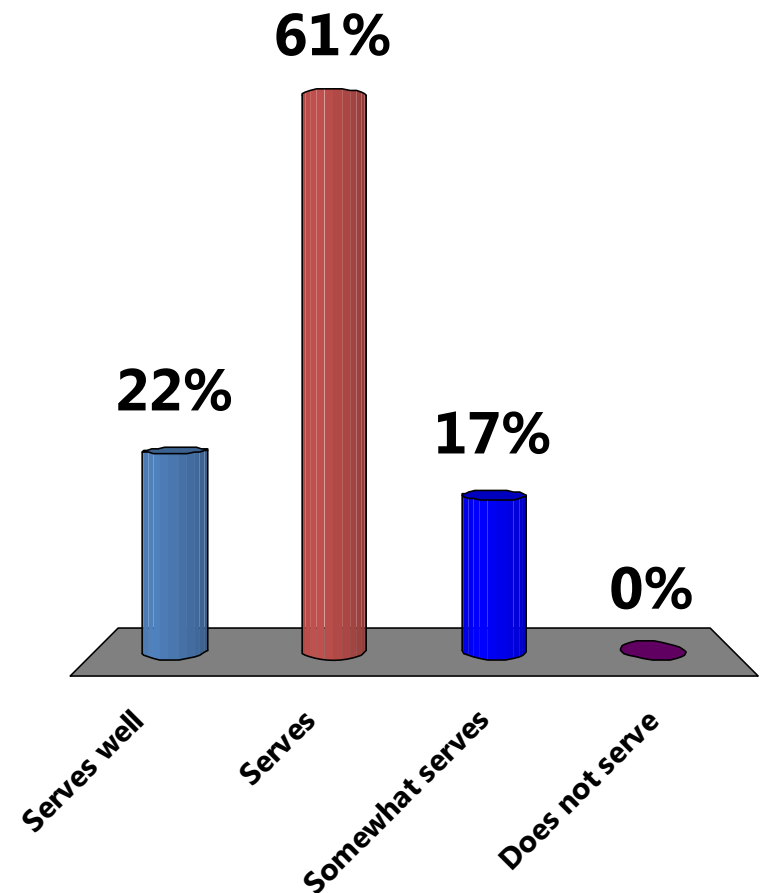
Group Exercise – Norfolk Southern Corridor

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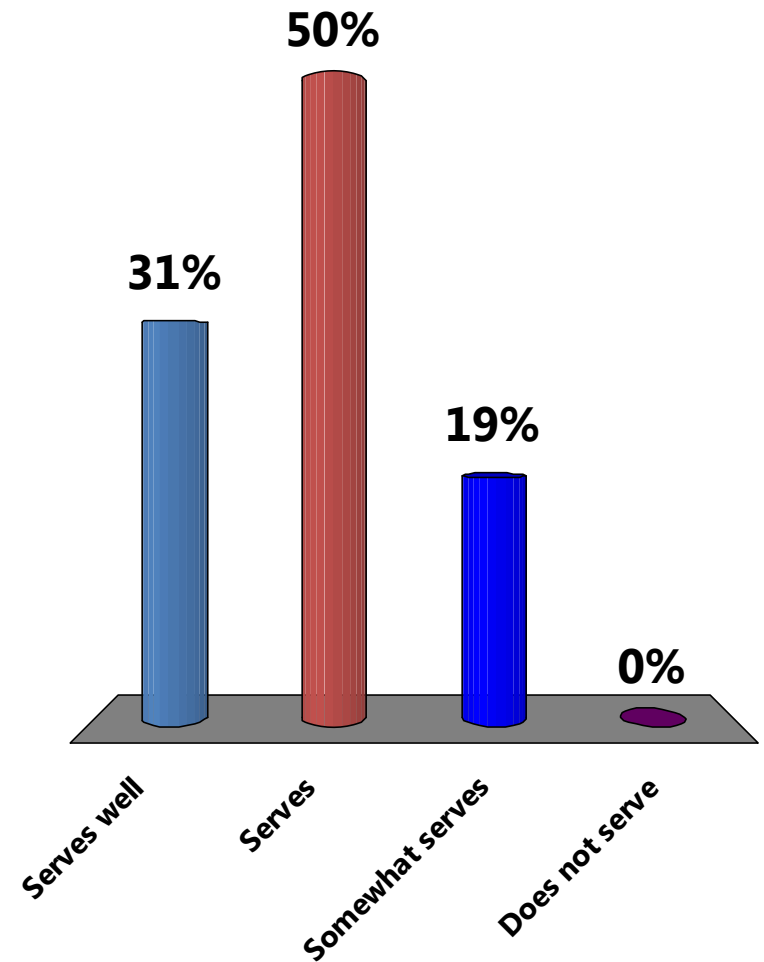
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Group Exercise - Norfolk Southern Corridor

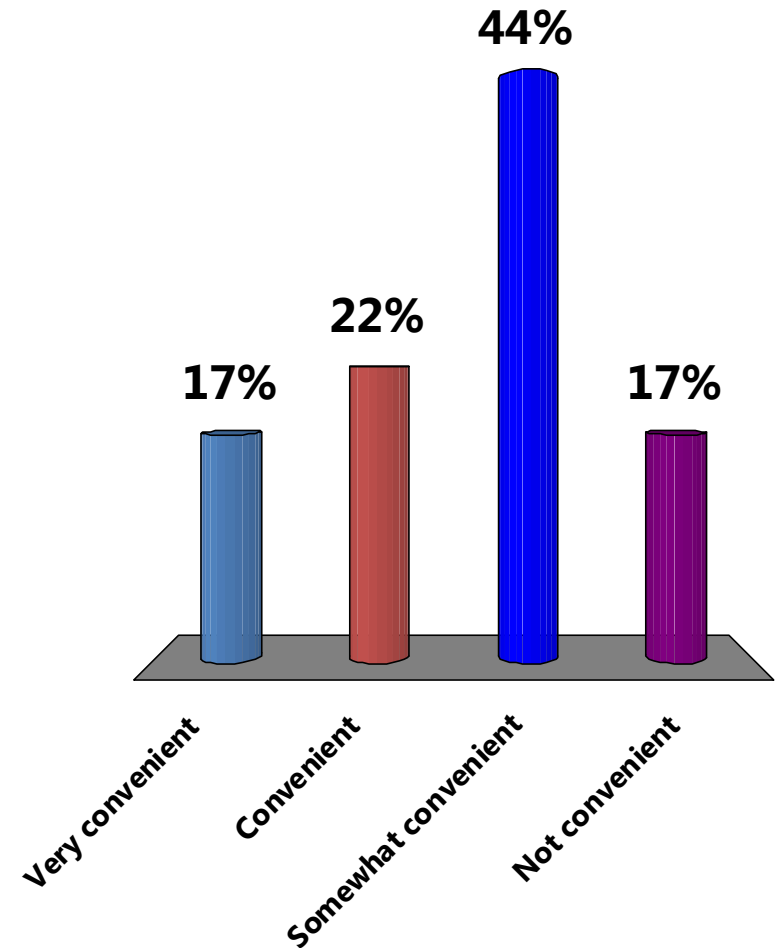
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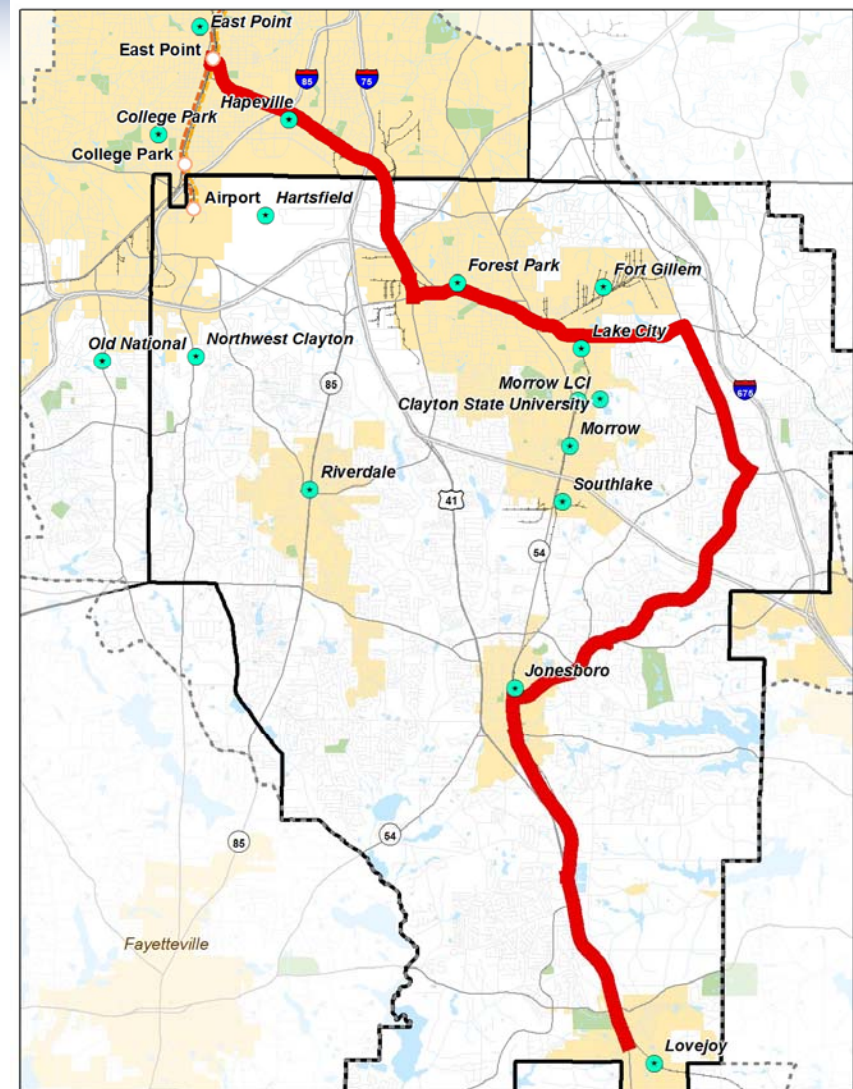
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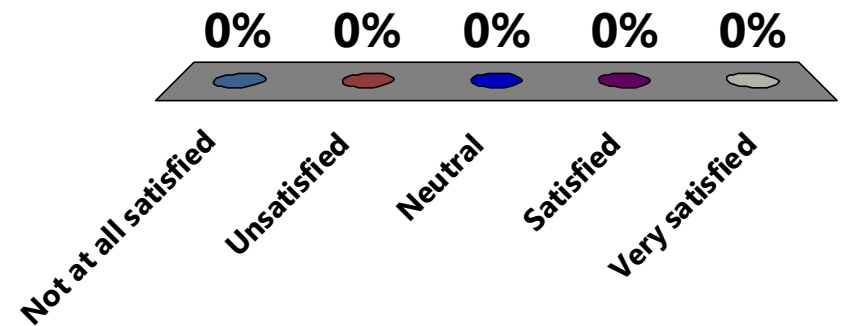
EASTSIDE CORRIDOR

- MARTA East Point Station
 - Central Ave
 - Old Dixie Rd
 - Forest Pkwy
 - Moreland Ave
 - Fielder Rd
 - Main St
 - Tara Blvd
- Lovejoy



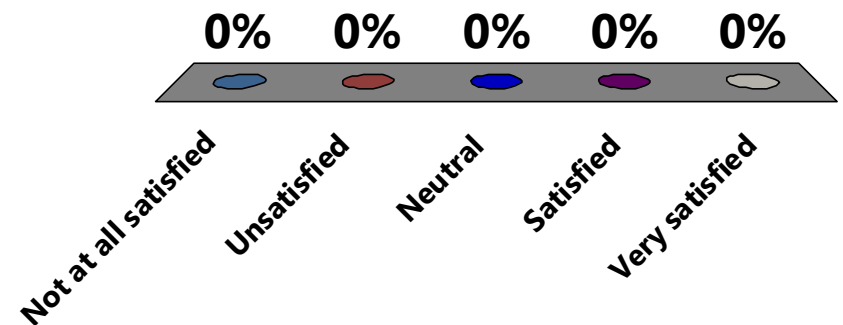
Group Exercise – Eastside Corridor

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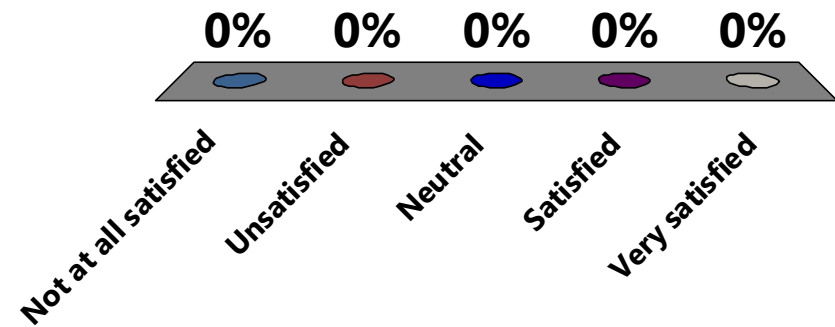
Group Exercise - Eastside Corridor

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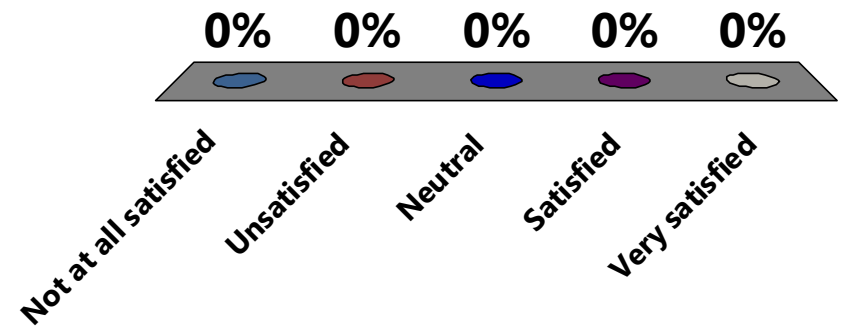
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Group Exercise - Eastside Corridor

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Next Steps

Next Steps

- Conduct Early Scoping Public Meetings
 - February 2, 2017 – 6:00 p.m.- 8:00 p.m.
Forest Park Community Center
 - February 9, 2017 – 6:00 p.m.- 8:00 p.m.
Frank Bailey Senior Center
- Finalize Purpose and Need
- Evaluate Potential Transit Alternatives
- Determine a Locally Preferred Alternative (LPA)

Questions?

